



The Maryland-National Capital Park and Planning Commission
 Prince George's County Planning Department
 Development Review Division
 301-952-3530

Detailed Site Plan

DSP-20035

Waste Management Capitol Facility

REQUEST	STAFF RECOMMENDATION
Construction of a 25,106-square-foot building for a Trash Maintenance Services with Private Fueling Station use.	APPROVAL with conditions

Location: On the east side of the intersection of D'Arcy Road and Sansbury Road.	
Gross Acreage:	7.77
Zone:	I-1/I-2/M-I-O
Dwelling Units:	N/A
Gross Floor Area:	25,659 sq. ft.
Planning Area:	78
Council District:	06
Election District:	15
Municipality:	N/A
200-Scale Base Map:	204SE08
Applicant/Address: Waste Management of Maryland, Inc. 6994 Columbia Gateway Drive, Suite 200 Columbia, MD 21046	
Staff Reviewer: Jeremy Hurlbutt Phone Number: 301-952-4277 Email: Jeremy.Hurlbutt@ppd.mncppc.org	



Planning Board Date:	01/28/2021
Planning Board Action Limit:	02/14/2021
Staff Report Date:	01/12/2021
Date Accepted:	10/14/2020
Informational Mailing:	07/14/2020
Acceptance Mailing:	10/01/2020
Sign Posting Deadline:	12/29/2020

The Planning Board encourages all interested persons to request to become a person of record for this application. Requests to become a person of record may be made online at http://www.mncppcapps.org/planning/Person_of_Record/. Please call 301-952-3530 for additional information.

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THE MARYLAND-NATIONAL CAPITAL
PARK AND PLANNING COMMISSION

PRINCE GEORGE'S COUNTY PLANNING BOARD

STAFF REPORT

SUBJECT: Detailed Site Plan DSP-20035
Type 2 Tree Conservation Plan TCP2-035-2020
Waste Management Capitol Facility

The Urban Design staff has completed the review of the subject application and appropriate referrals. The following evaluation and findings lead to a recommendation of APPROVAL with conditions, as described in the Recommendation section of this report.

EVALUATION

The detailed site plan was reviewed and evaluated for conformance with the following criteria:

- a. The requirements of the Prince George's County Zoning Ordinance for the Light Industrial (I-1), Heavy Industrial (I-2), and Military Installation Overlay (M-I-O) Zones and site design guidelines;
- b. The requirements of the 2010 *Prince George's County Landscape Manual*;
- c. The requirements of the Prince George's County Woodland and Wildlife Habitat Conservation Ordinance;
- d. The requirements of the Prince George's County Tree Canopy Coverage Ordinance; and
- e. Referral comments.

FINDINGS

Based upon the evaluation and analysis of the subject application, the Urban Design Section recommends the following findings:

1. **Request:** The subject application is for approval of a detailed site plan (DSP) to raze the existing 5,628-square-foot building and construct a 25,106-square-foot administrative building with an eight bay, drive-through, commercial fleet service and shop area for a Trash Maintenance Services with Private Fueling Station use.

2. Development Data Summary:

	EXISTING	PROPOSED
Zone	I-1, I-2, M-I-O	I-1, I-2, M-I-O
Use	Community Trailer, Fleet Maintained Recycled Metal Scrap Yard	Community Trailer, Trash Maintenance Services with Private Fueling Station
Total Acreage	7.77	7.77
Green Area (10 percent required)		3.20 acres/42 percent
Parcels	1	1
Total Gross Floor Area (sq. ft.)	6,181	25,659
Community Trailer (sq. ft.)	553	553
Industrial/Office (sq. ft.)	5,628 (to be razed)	25,106

OTHER DEVELOPMENT DATA

Use	No. of Spaces Required	No. of Spaces Provided
Office Space –10,396 sq. ft. @ 1 space/250 sq. ft. for first 2,000 sq. ft.	8	8
8,396 sq. ft. additional @ 1 space per 400 sq. ft.	21	21
Vehicle Repair and Service Station – 8 service bays @ 3 spaces per bay	24	24
Total Parking Spaces	53	96
Handicapped-Accessible	3	4
CNG Fueling Spaces		93*
Total Loading Spaces	2	2**
Industrial @ 1 space per 2,000 to 25,000 sq. ft.	1	1
10,396 sq. ft. of Office @ 1 space/10,000 to 100,000 sq. ft.	1	1

Notes: *Compressed natural gas system (CNG) fueling spaces allow for refueling of trucks and overnight storage, but do not count towards required parking.

**An additional loading space is required per Section 27-582 of the Zoning Ordinance and has been conditioned in the Recommendation section.

3. Location: The subject property is located on the east side of the intersection of D’Arcy Road and Sansbury Road in Planning Area 78, Council District 6. The subject property is a single parcel, located on Tax Map 82 in Grids D-3 and E-3, and identified as Parcel 202, described by deed recorded in Liber 17657 at folio 670. The property is in the 2007 *Approved Westphalia Sector Plan and Sectional Map Amendment* (Westphalia Sector Plan and SMA). More specifically, the site is located at 9304 D’Arcy Road.

4. Surrounding Uses: The site is bounded by industrial uses to the north and south in the Light Industrial (I-1) and Heavy Industrial (I-2) Zones. D’Arcy Road is to the west and beyond are residential uses in the Rural Residential Zone. An active gravel mine is located to the east in the Residential-Agricultural Zone.

5. **Previous Approvals:** The property is subject to Preliminary Plan of Subdivision (PPS) 4-19035, which was approved by the Prince George's County Planning Board on May 14, 2020 (PGCPB Resolution No. 2020-79(C)) subject to 14 conditions. The PPS approved one parcel for 25,659 square feet of industrial/institutional use.

On November 17, 2020, Prince George's County Council Bill CB-63-2020 was adopted for the purpose of amending the Prince George's County Zoning Ordinance to provide a definition for a new use, "Trash Maintenance Services with Private Fueling Station," and permitting the use in the I-1 and I-2 Zones under certain circumstances. The effective date of CB-63-2020 was January 4, 2021, and this DSP is proposing this new use.

A Stormwater Management (SWM) Concept Approval Letter (25803-2019-01) and associated plan were submitted with the application for this site. The approval was issued on May 14, 2020 for this project from the Prince George County Department of Permitting, Inspections and Enforcement (DPIE). The plan proposes to construct 24 micro-bioretenion ponds, a submerged gravel wetland, and an undetained pervious area. No SWM fee for on-site attenuation/quality control measures is required.

6. **Design Features:** The site will have dual access points from D'Arcy Road, with three bays of surface parking for employees, in front of the proposed building. The front of the building will be 10,396 square feet of office with eight drive-through maintenance bays in the rear. Behind the building are 1 loading space and 93 compressed natural gas system fueling spaces for fleet vehicles, which will also serve as overnight fleet storage. Two large landscape islands will define circulation in the fleet area and house SWM facilities. Mechanical equipment and natural gas storage tanks will be located near the northern property line in the middle of the property. A diesel fuel tank will be located at the eastern end of the fleet parking area and will allow two trucks to fuel at a time with a bypass lane. The far east end of the property will be impervious area with a stormwater pond and woodland conservation areas. A 553-square-foot community trailer will be retained on the site in the northwest corner of the property.

Architecture

The 29-foot-high steel industrial clear span building is generally rectangular and includes a low gable roof. The building will have brick veneer on three sides of the front two stories where the office will be located. Masonry wainscot, steel siding, and standing seam metal roof will be used to complete the façade. The applicant proposes earth tones with a green band of color that runs along the top of the brick water table, corners of the building, roof line, and metal entrance canopies. The rest of the building will be painted an off-white color. Windows on the two-story office section and overhead garage doors will break up the sides and rear of the building.



Figure 1: Rendering of the southwest corner of the building

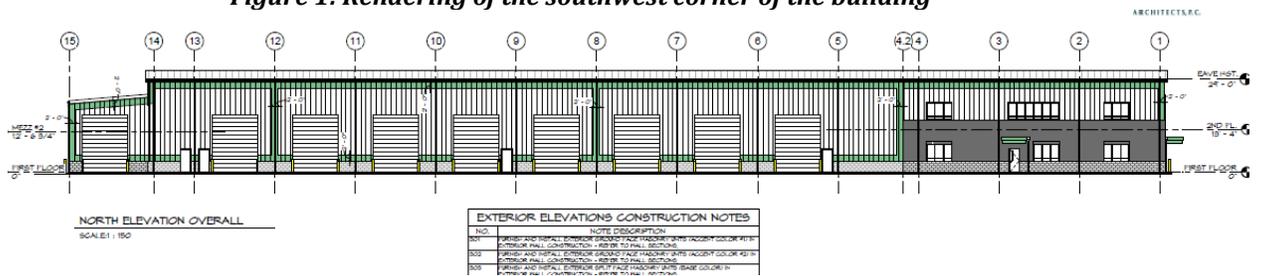


Figure 2: Proposed north elevation

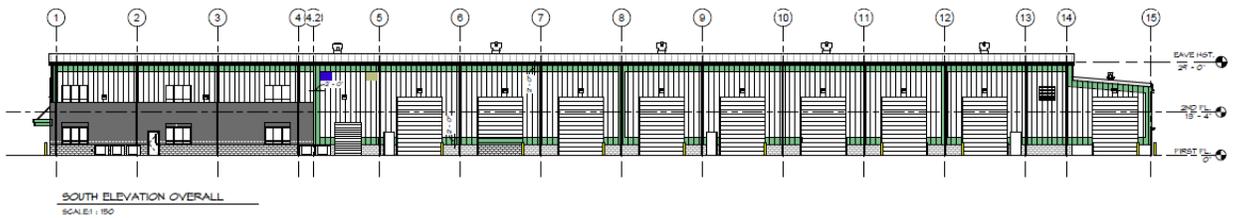
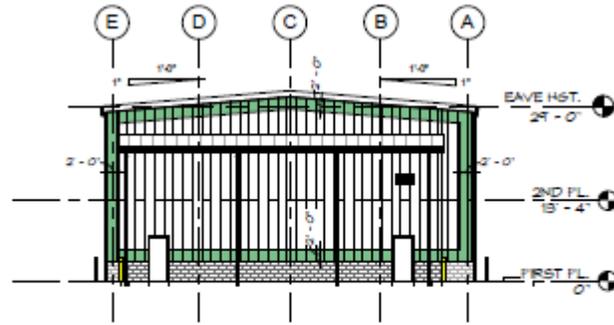


Figure 3: Proposed south elevation



EAST ELEVATION OVERALL

SCALE: 1/150

Figure 4: Proposed east elevation



WEST ELEVATION OVERALL

SCALE: 1/150

Figure 5: Proposed west elevation

Lighting

This DSP proposes light-emitting diode lighting on the building and within the parking area to illuminate the building, parking areas, pedestrian walkways, and loading spaces on the site. The site plans show pole-mounted lighting in the parking area and a variety of building-mounted lighting to provide a balanced lighting pattern on the property. Lighting has been placed to highlight building entrances and to provide employees with a bright and safe atmosphere, while not causing a glare onto adjoining properties, as referenced in the photometric plan that was submitted with this application. It is noted that the details of the lighting types and models have been shown, but the application does not specify the height or detail for the proposed pole.

Signage

One building-mounted sign and one freestanding sign are proposed, which are in conformance with Sections 27-613 and 27-614 of the Zoning Ordinance. The 32.38-square-foot building-mounted sign, which includes letters and logo in green and yellow advertising the company name, will be located on the upper story of the west façade facing D'Arcy Road. A 49-square-foot freestanding sign is proposed. The location of the freestanding sign and materials and illumination have not been provided and a condition has been added to the Recommendation section requiring the applicant to provide this information.



Figure 6: Proposed building-mounted signage

COMPLIANCE WITH EVALUATION CRITERIA

7. **Prince George’s County Zoning Ordinance:** This application has been reviewed for compliance with the requirements of the Zoning Ordinance in the I-1 Zone and the site plan design guidelines.
- a. This DSP is in general conformance with the requirements of Section 27-473(b) of the Zoning Ordinance, which governs uses in industrial zones. A Trash Maintenance Services with Private Fueling Station use, as is proposed, is defined as a business involving the maintenance, dispatch, and storage of trucks and dumpsters for the purpose of trash removal and contains an on-site private fueling station. The use does not include any retail sale of gasoline or compressed natural gas.

In the I-1 and I-2 Zones, this use is permitted subject to Footnote 76, which requires conformance to Section 27-475.06.10(a) of the Zoning Ordinance, which lists the following requirements:

(1) Requirements.

- (A) Trash collected by this business shall not be brought to or stored upon the property;**

The applicant has stated that trash will not be stored on-site, and the property will be for fleet storage, maintenance, and fueling only.

- (B) The use is located on property with a preliminary plan of subdivision approved prior to September 1, 2020;**

PPS 4-19035 was approved by the Planning Board on May 14, 2020.

- (C) The applicable Master Plan or Sector Plan recommends an industrial zone and/or use for the property;**

The Westphalia Sector Plan and SMA recommends industrial land uses on the subject property.

(D) The use is located on property within the M-I-O Zone;

This application is located within the Imaginary Surface E (Conical Surface) of the Military Installation Overlay Zone. Pursuant to Section 27-548.54, Requirements for Height, of the Zoning Ordinance, the maximum height for structures in this area is 495 feet. The proposed building conforms to this requirement.

(E) The use is located on property with a land area greater than seven (7) acres;

The property is located on a parcel of land that is 7.77 acres in size.

(F) The use is located on property with frontage on a roadway with a functional transportation classification of collector; and

The subject property has frontage on D'Arcy Road, which is a master-planned collector right-of-way.

(G) The property abuts at least one parcel of land located, wholly or in part, in the I-2 Zone.

The property is split-zoned, with the center portion being located in the I-2 Zone and remainder in the I-1 Zone. The adjacent properties to the north and south are also zoned I-2 in part.

(2) Site Plan.

(A) A Detailed Site Plan shall be approved for the use, in accordance with Part 3, Division 9, of this Subtitle, to ensure compliance with the provisions of this Section.

The subject DSP was filed in compliance with this requirement.

(B) In addition to the requirements of Part 3, Division 9, the following requirements shall be complied with:

(i) The applicant shall comply with the Prince George's County Landscape Manual; and

2010 Prince George's County Landscape Manual (Landscape Manual) conformance is discussed in Finding 9 below.

(ii) The fueling equipment and fuel storage maintains a minimum setback of twenty (20) feet from all property lines.

This setback is demonstrated on the site plan.

- (iii) **The use shall conform with the requirements of Section 27-358(a)(2) and (c) of this Subtitle, more specifically, that upon the abandonment of the Trash Maintenance Services with Private Fueling Station, the Private Fueling Station shall terminate and all structures exclusively used for the Private Fueling Station (including underground storage tanks), except buildings, shall be removed by the owner of the property. For the purpose of this Subsection, the term "abandonment" shall mean nonoperation as a gas station for a period of fourteen (14) months after the retail services cease.**

This requirement will be applied upon abandonment of the use.

- (iv) **Fueling equipment associated with the use shall not be located less than one hundred(100) feet from the primary ingress/egress of the property.**

This setback is demonstrated on the site plan.

- b. The DSP is consistent with those regulations in the I-1 and I-2 Zones, including Sections 27-469(a) and 27-470(a) of the Zoning Ordinance, regarding purposes; Sections 27-469(b) and 27-470(b) regarding landscaping and amount of green area on-site; and Section 27-474, of the Zoning Ordinance, regarding regulations in the industrial zones.
- c. The DSP is in general conformance with the applicable site design guidelines, as referenced in Section 27-283 and contained in Section 27-274 of the Zoning Ordinance. For example, vehicular and pedestrian circulation is designed to be safe, efficient, and convenient for both pedestrians and drivers; pedestrian access is provided to the site from the public right-of-way; the architecture proposed for the building is constructed of durable, low-maintenance materials; and the new green area incorporates a significant amount of landscaping that greatly improves the site conditions.

- 8. **Preliminary Plan of Subdivision 4-19035:** PPS 4-19035 was approved by the Planning Board on May 14, 2020 (PGCPB Resolution No. 2020-79(C)), subject to 14 conditions. The PPS approved one parcel for 25,659 square feet of industrial/institutional use. Of the 14 conditions of approval of PPS 4-19035, 7 are applicable to the subject DSP, as follows:

- 2. **A substantial revision to the mix of uses on the subject property that affects Subtitle 24 adequacy findings shall require approval of a new preliminary plan of subdivision prior to issuance of any permits.**

The DSP proposal regarding land use is consistent with the approved PPS. The proposed industrial use was identified as a use for Parcel 1 during the PPS review process. Conformance with this condition has been demonstrated.

3. **Development of this site shall be in conformance with an approved stormwater management concept plan and any subsequent revisions. Prior to approval, the final plat shall note the stormwater concept approval number.**

The applicant submitted a copy of approved SWM Concept Plan 25803-2019-01, which shows conceptual SWM for the proposed development. This condition will be met at the time of final plat.

4. **Prior to approval of a final plat:**

- a. **The final plat shall include the grant of a 10-foot-wide public utility easement along the public right-of-way.**

A 10-foot-wide public utility easement (PUE) is delineated on the DSP along only one portion of the proposed right-of-way of D'Arcy Road. The PUE is incorrectly shown extending into the existing public road right-of-way.

The applicant has stated that the County intends to abandon a portion, measuring 540 square feet in area, of existing public right-of-way for D'Arcy Road, which was previously dedicated from existing Parcel 202 by deed. This 540 square foot area is part of a previously conveyed 4,618-square-foot right-of-way to Prince George's County via deed recorded in the County Courts in Liber 24843 at folio 756, dated April 11, 2006. The process of abandonment of public land and conveyance to private property will occur independent of the permitted development of this property. Since the ultimate right-of-way line for D'Arcy Road is expected to change after inclusion of the conveyed acreage into the subject property, the applicant does not intend to show PUE dedication along the existing property line segments labeled as "L4" and "C2" on the DSP. The applicant intends to submit a variation from Section 24-122(a) of the Prince George's County Subdivision Regulations, at the time of final plat, from providing a 10-foot-wide PUE along this portion of the property fronting D'Arcy Road. The location of PUE's and the variation will be further evaluated at the final plat stage.

- b. **The final plat shall include the necessary dedication of right-of-way for D'Arcy Road as determined by the Department of Permitting, Inspections and Enforcement.**

In May 2020, per correspondence between Ms. Rodgers of Lawson Design Group and Mr. Mariwan Abdullah of DPIE, DPIE expressed agreement with the applicant's proposal, which showed a 32-foot right-of-way width from the proposed roadway centerline of D'Arcy Road. D'Arcy Road is planned to be straightened along the frontage of the subject property, and the DSP shows the proposed centerline of the re-aligned road and the 32-foot right-of-way width as agreed by DPIE. In addition, a proposed public right-of-way dedication for 1,012 square feet along D'Arcy Road is shown on the DSP.

- 8. Prior to acceptance of a detailed site plan submission, an invasive species management plan shall be included as part of the application on the Type 2 tree conservation plan.**

An invasive species management plan is included in Type 2 Tree Conservation Plan TCP2-035-2020.

- 9. In conformance with the 2009 Approved Countywide Master Plan of Transportation, the applicant and the applicant's heirs, successors, and/or assignees shall provide the following:**

- a. A shared-lane marking (sharrow) on D'Arcy Road, along the frontage of the subject site, unless modified by the Prince George's County Department of Permitting, Inspections, and Enforcement with written correspondence.**
- b. Two inverted-U style bicycle racks at a location that is convenient to the entrance of the building.**

The applicant's submission displays a shared-lane marking (sharrow) on D'Arcy Road, as well as two bicycle racks located at the entrance of the facility. Staff recommends the applicant include a detail sheet displaying the bicycle rack as an inverted U-style, or a similar style that will provide two points of contact to support and secure a parked bicycle, with specifications and dimensions provided.

- 12. Total development within the subject property shall be limited to uses which generate no more than 44 AM peak-hour trips and 46 PM peak-hour vehicle trips. Any development generating an impact greater than that identified herein above shall require a new preliminary plan of subdivision with a new determination of the adequacy of transportation facilities.**

The site plan proposes a building with the same mix of uses with a slightly smaller gross floor area than the facility on which the PPS was approved. Staff therefore concludes that the development represented by this plan will not generate more traffic than was anticipated in the PPS, consequently, the trip cap established in Condition 12 of the approved PPS will not be exceeded.

- 9. 2010 Prince George's County Landscape Manual:** The application is subject to Section 4.2, Requirements for Landscape Strips Along Streets; Section 4.3, Parking Lot Requirements; Section 4.4, Screening Requirements; Section 4.7, Buffering Incompatible Uses; and Section 4.9, Sustainable Landscaping Requirements, of the Landscape Manual. The required plantings and schedules are provided, in conformance with the Landscape Manual, with the exception of Section 4.2. Initially, the applicant requested alternative compliance (AC) from Section 4.2, Requirements for Landscape Strips Along Streets, along D'Arcy Road. However, the applicant was providing extra wide driveways in the front parking lot. Staff recommends that the parking lot be revised so the AC request is not required, as conditioned herein.

- 10. Prince George’s County Woodland and Wildlife Habitat Conservation Ordinance:** This property is subject to the provisions of the Woodland and Wildlife Habitat Conservation Ordinance (WCO) because it has previously approved TCPs. TCP2-035-2020 was submitted with the DSP application.

According to the worksheet shown on the TCP2 as submitted, the site is 7.77 acres split-zoned between the I-1 (5.12 acres) and I-2 (2.65 acres) Zones. A total of 0.38 acre of existing woodland is on the net tract, and has a woodland conservation threshold of 1.17 acres, or 15 percent of the net tract, as tabulated. The Woodland Conservation Worksheet proposes the removal of 0.02 acre in the net tract area, for a woodland conservation requirement of 1.19 acres. The TCP2 shows this requirement will be met by providing 0.36 acre of woodland preservation on-site, 0.37 acre of on-site reforestation, 0.13 acre of natural regeneration on-site, and 0.33 acre of off-site woodland conservation credits.

The TCP2 requires additional technical corrections to be in conformance with the WCO. These revisions are specified in the recommended conditions below.

- 11. Prince George’s County Tree Canopy Coverage Ordinance:** Subtitle 25, Division 3, the Tree Canopy Coverage Ordinance requires a minimum percentage of tree canopy coverage (TCC) on projects that require a grading permit for 5,000 square feet or greater. Properties that are within the I-1 and I-2 Zones are required to provide a minimum of 10 percent of the gross tract area in TCC. This project has 7.77 acres that results in a required TCC of 0.78 acre for the site, or 33,803 square feet. Staff notes that the plans propose more than what is required and meet the requirements of the Tree Canopy Coverage Ordinance.

- 12. Referral Comments:** The subject application was referred to the appropriate County agencies and divisions. The referral comments are summarized, as follows:

- a. **Historic Preservation**—In a memorandum dated October 19, 2020 (Stabler to Bishop), incorporated herein by reference, the Historic Preservation Section noted that in a search of current and historic photographs, topographic and historic maps, and locations of currently known archeological sites indicates the probability of archeological sites within the subject property is low. The subject property does not contain and is not adjacent to any designated Prince George’s County Historic Sites or resources.
- b. **Community Planning**—In a memorandum dated November 5, 2020 (Gravitz to Hurlbutt), incorporated herein by reference, the Community Planning Division indicated that, pursuant to Part 3, Division 9, Subdivision 3, of the Zoning Ordinance, master plan conformance is not required for this DSP.
- c. **Subdivision**—In a memorandum dated December 23, 2020 (Gupta to Hurlbutt), incorporated herein by reference, the Subdivision Section provided an evaluation of the application that is incorporated into Finding 8 and found the application acceptable.
- d. **Trails**—In a memorandum dated December 8, 2020 (Ryan to Hurlbutt), incorporated herein by reference, the Trails planner provided an evaluation for conformance with the applicable conditions of the PPS, in order to implement

planned bikeways and pedestrian improvements. Staff found the pedestrian and bicycle transportation site access and circulation acceptable.

- e. **Transportation Planning**—In a memorandum dated December 5, 2020 (Burton to Hurlbutt), incorporated herein by reference, the Transportation Planning Section noted that the site was reviewed for conformance with previous conditions of approval. The subject application is proposing two access points on D’Arcy Road, a master-planned collector road (C-627) within 80 feet of right-of-way. The site plan is showing additional right-of-way to be dedicated, that is consistent with the master plan requirement. Overall, from the standpoint of transportation, it is determined that this plan is acceptable and meets the findings required for a DSP.
 - f. **Environmental Planning**—In a memorandum dated December 28, 2020 (Rea to Hurlbutt), incorporated herein by reference, the Environmental Planning Section provided an analysis of the previous conditions of approval and the application. The Environmental Planning Section noted the site has an approved Natural Resources Inventory Plan NRI-077-2019. The site is the subject to SWM Concept Approval Letter 25803-2019, that proposes to construct 24 micro-bioretenion ponds, a submerged gravel wetland, and an undetained pervious area. There are no regulated environmental features located on the subject site or the adjacent parcels; however, the Soil Conservation District may require additional or redundant erosion and sediment control devices.
 - g. **Prince George’s County Fire/EMS Department**—As of the writing of this staff report, the Prince George’s County Fire/EMS did not offer any comments on the subject application.
 - h. **Prince George’s County Department of Permitting, Inspections and Enforcement (DPIE)**—As of the writing of this staff report, DPIE did not offer any comments on the subject application.
 - i. **Prince George’s County Police Department**—In a memorandum dated October 29, 2020 (Contic to Hurlbutt), incorporated herein by reference, the Police Department indicated that upon review of these site plans, they have no comments at this time.
 - j. **Prince George’s County Health Department**—As of the writing of this staff report, the Health Department did not offer any comments; however, standard conditions have been included in the Recommendation section of this report to require noise and dust control during the demolition and construction phases of the development.
 - k. **Washington Suburban Sanitary Commission (WSSC)**—As of the writing of this staff report, WSSC did not offer any comments on subject application.
13. As required by Section 27-285(b)(1) of the Zoning Ordinance, this DSP, if approved with the recommended conditions, represents a reasonable alternative for satisfying the site design guidelines of Subtitle 27, Part 3, Division 9, of the Prince George’s County Code without requiring unreasonable costs, and without detracting substantially from the utility of the proposed development for its intended use.

14. As required by Section 27-285(b)(4), the Planning Board must also find that the regulated environmental features on a site have been preserved and/or restored in as natural a state as possible, in accordance with the requirements of Subtitle 24-130(b)(5) of the Subdivision Regulations. The site does not contain any regulated environmental features that are required to be protected. Therefore, this finding is not applicable to this DSP.

RECOMMENDATION

Based upon the foregoing evaluation and analysis, the Urban Design staff recommends that the Prince George's County Planning Board adopt the findings of this report and APPROVE Detailed Site Plan DSP-20035 and Type 2 Tree Conservation Plan TCP2-035-2020 for Waste Management Capitol Facility, subject to the following conditions:

1. Prior to certification of the detailed site plan, the following revisions shall be made to the plans:
 - a. Correct the gross floor area for proposed Parcel 1 after excluding the 540 square feet of previously conveyed public right-of-way.
 - b. Correct the proposed 10-foot-wide public utility easement along D'Arcy Road to end at the property line.
 - c. Provide the square footage of the green space in the table on the cover sheet.
 - d. Correct the required parking to 53 spaces.
 - e. Reduce parking lot driveways from 24 to 22 feet and enlarge the Section 4.2 landscape strip along D'Arcy Road.
 - f. Modify the notes and parking calculations to show the nine drive-through bays.
 - g. Label the height of the clearance provided in the drive through opening on the architectural elevations.
 - h. Provide the location, square footage, lighting, and materials of the freestanding sign on the plans, in conformance with Section 27-614 of the Prince George's County Zoning Ordinance.
 - i. Indicate the lighting and materials of the building-mounted sign on the plans.
 - j. Correct adjacent property information on the landscape plan to show that the properties to the north and south are in the Light Industrial and Heavy Industrial Zones, and that the property to the east is zoned Residential-Agricultural.
 - k. Provide one additional loading space, in conformance with Section 27-582 of the Prince George's County Zoning Ordinance.

- l. Add the following general notes:
 - (1) During the demolition/construction phases of this project, no dust should be allowed to cross over property lines and impact adjacent properties. Conformance to construction activity dust control requirements, as specified in the 2011 Maryland Standards and Specifications for Soil Erosion and Sediment Control, is required.
 - (2) During the demolition/construction phases of this project, noise should not be allowed to adversely impact activities on the adjacent properties. Conformance to construction activity noise control requirements, as specified in Subtitle 19 of the Prince George's County Code, is required.

- m. Revise the Type 2 tree conservation plan (TCP2), as follows:
 - (1) Add TCP2-035-2020 to all approval blocks on the TCP2.
 - (2) Remove the part of the note in the Invasive Species Management Plan regarding qualifying for additional woodland conservation credit for the reduction of invasive species in the herbaceous layer.
 - (3) Change the title block on all pages to reflect this as a TCP2 plan not a TCP1 plan.
 - (4) Add TCP2-035-2020 to the project name of the Invasive Species Management Plan.

2. Prior to the certification of the Type 2 tree conservation plan (TCP2) for this site, documents for the required woodland conservation easements shall be prepared and submitted to the Environmental Planning Section for review by the Office of Law, and submission to the Prince George's County Land Records or recordation. The following note shall be added to the standard TCP2 notes on the plan, as follows:

"Woodlands preserved, planted, or regenerated in fulfillment of woodland conservation requirements on-site have been placed in a woodland and wildlife habitat conservation easement recorded in the Prince George's County Land Records at Liber ____ Folio ____. Revisions to this TCP2 may require a revision to the recorded easement."