

The Maryland-National Capital Park and Planning Commission
Prince George's County Planning Department
Countywide Planning Division
301-952-3650



Note: Staff reports can be accessed at www.mncppc.org/pgco/planning/plan.htm

Mandatory Referral

MR-1411F

Application	General Data	
Project Name: Proposed Modular Police Station Building – Seat Pleasant Location: 6011 Addison Road, Seat Pleasant, MD 20743 Applicant/Address: 6301 Addison Road, Seat Pleasant, MD 20743 Property Owner: City of Seat Pleasant	Planning Board Hearing Date:	10/23/14
	Date Accepted:	09/10/14
	Mandatory Action Timeframe:	60-Day Review
	Acreage:	2.16 acres
	Zone:	R-55
	Planning Area:	72/Landover and Vicinity
	General Plan Tier:	Established Communities
	Council District:	District 7
	Adjacent Municipality:	Seat Pleasant

Purpose of Application	Notice Date
Approval of a modular police station building to house the Seat Pleasant Police Force, support staff, and code enforcers.	Acceptance Mailing: September 29, 2014

Staff Recommendation	Staff Reviewer:
Transmit Staff Report to: Ms. LaTasha C. Gatling City Administrator City of Seat Pleasant 6301 Addison Road Seat Pleasant, MD 20743	Fatimah Hasan
	Phone Number: 301-952-3580
	Email: Fatimah.Hasan@ppd.mncppc.org

PROJECT SUMMARY

The City of Seat Pleasant proposes to utilize the approximately 15-foot high, 2,160 square foot modular building for the purpose of housing the city's police force, support staff, and code enforcers. In 1989, the police department had to be relocated due to the poor physical condition of the structure and due to overcrowding at the old station house at 410 Addison Road, which was demolished around that time. The police department then moved temporarily into a new Police Services Complex in Palmer Park. From there they moved to their current temporary location at 6264 Central Avenue, at the Addison Plaza Shopping Center.

The proposed permanent site that is the subject of this review is approximately 2.16 acres in size and is located in the southwest quadrant of the intersection of Addison Road and Eads Street. The address is 6011 Addison Road, Seat Pleasant, Maryland 20743.

The project required a full MR review due to the fact that it is a new structure that is proposed to be the permanent home of the police department, support staff, and code enforcers. Adequate pedestrian access as well as stormwater management and landscaping were among the issues that were discussed. Additional suggestions to improve the overall site design are identified in the memoranda attached to this staff report.

STAFF RECOMMENDATIONS

The proposed modular police station building project underwent a careful review by Planning Department staff, resulting in the following recommendations:

- The applicant should reuse existing pavement to reduce the need for further paving, remove unnecessary paving and use pervious paving for all new pavement needed.
- The applicant is encouraged to include a direct pedestrian connection from the building to the sidewalk.
- Aesthetics of the building should be enhanced through the incorporation of additional materials and colors to add visual interest, such as the addition of shutters and other architectural details to emphasize existing doors and windows; the use of enhanced cornice treatments to improve the roofline; and the addition of decorative, native, landscape plantings to provide better integration of the building into the site and screen the mechanical equipment on the southern façade. These improvements will provide a more visually and architecturally attractive building, inspiring civic pride and commanding respect for the important policing functions that take place within its walls.
- The applicant should incorporate Leadership in Energy and Environmental Design (LEED) techniques in the design and functioning of the new building, such as encouraging recycling of office trash, usage of low-flow water fixtures, installation of a programmable thermostat, and using lights with sensor switches.
- The proposed building should be enhanced by the incorporation of site improvements such as a public plaza or gathering space, including specialty paving, seating and landscaping and an aesthetically pleasing identification sign, constructed of high quality architectural materials.
- The site should include two U-shaped bicycle parking spaces anchored into a concrete base.
- Because the site is not graded for accessible pedestrian access, it is recommended that a pedestrian staircase be constructed to provide safe access for pedestrians from Addison Road to the building. This recommendation is subject to modification and approval by the Department of Permitting, Inspections and Enforcement (DPIE).
- It is recommended that vehicle parking spaces be striped according to County standards and specifications, and that American with Disabilities Act (ADA) accessible parking spaces be provided at amounts that are prescribed in the zoning ordinance for similar uses.

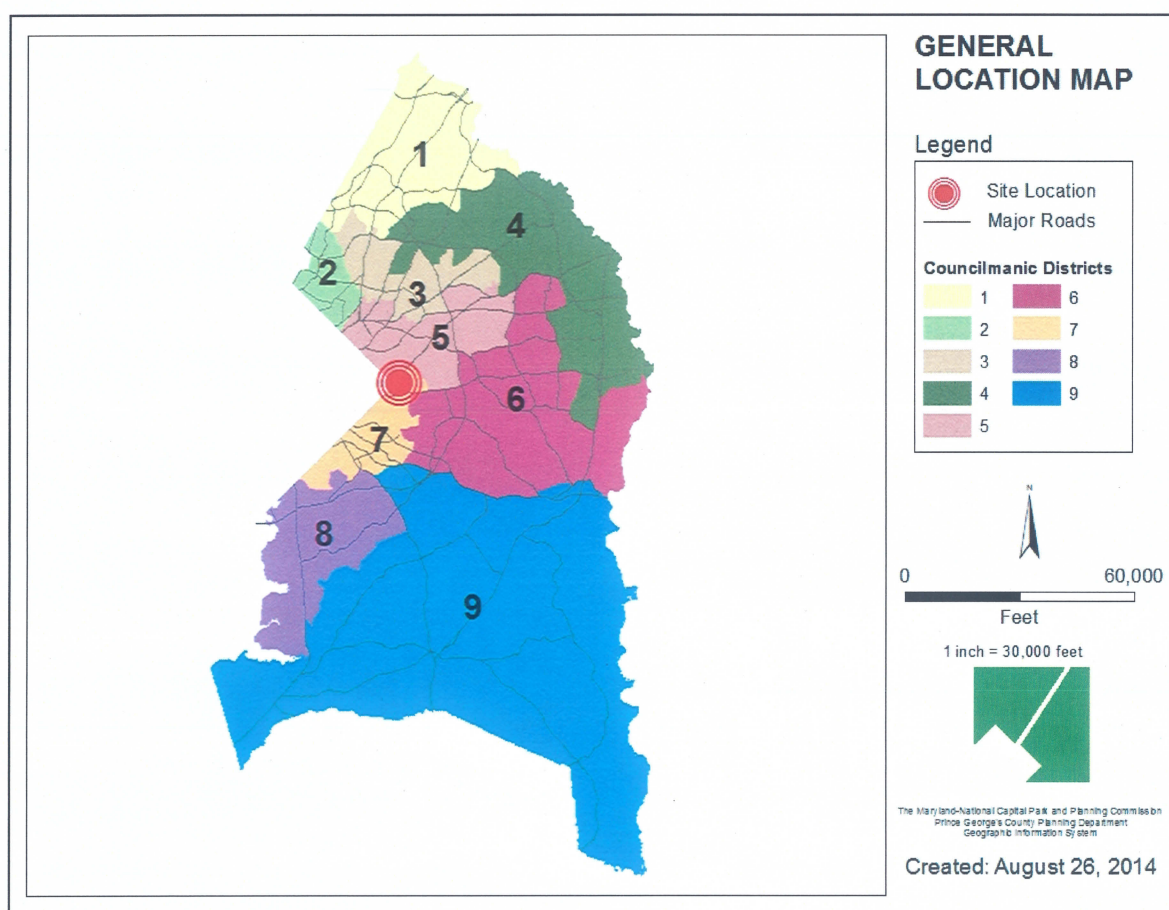
- The project qualifies for a standard exemption from the Woodland and Wildlife Habitat Conservation Ordinance because the site contains less than 40,000 square feet of existing woodland. The applicant must obtain an exemption letter prior to the issuance of any local permit for the site.
- The project qualifies for a Natural Resource Inventory Equivalency Letter because the application qualifies for a standard exemption from the Prince George's County Woodland and Wildlife Habitat Conservation Ordinance since no regulated environmental features are located on-site. The applicant must obtain an equivalency letter for the County's Stormwater Management application process.
- All construction is required to be conducted in accordance with erosion control and stormwater runoff laws and regulations to prevent any adverse effects on water quality. The site plan submitted with the application shows the use of on-site bioretention for stormwater management purposes. The stormwater management design is required to be reviewed and approved locally by the Department of Permitting, Inspections and Enforcement (DPIE) and a determination of the use of Environmental Site Design, to the maximum extent practicable in accordance with current state regulations, will be made with the DPIE approval. The Erosion and Sediment Control design is required to be reviewed and approved by the Prince George's County Soil Conservation District.

PROJECT LOCATION

Situated on the Prince George's County/District of Columbia border, the City of Seat Pleasant was incorporated in 1931, and currently has a population of approximately 4,500 persons. Its location is shown on Map 2. Martin Luther King, Jr. Highway (MD 704) and Central Avenue (MD 214) run through Seat Pleasant. The site is approximately 0.68 miles from the Addison Road Metro Station in an area of projected future growth.

The 2.16-acre property is located near other city-owned properties. West of the site is vacant city-owned property, and south of the site is a property developed as a passive park. To the east and north are single-family detached homes that are zoned R-55, One Family Detached Residential Zone (see Map 1). The property is in the R-O-S (Reserved Open Space) Zone, (shown on Map 3), which includes areas of very low density residential development and a limited range of public, recreational, and agricultural uses.

Map 2 – Proposed Modular Police Station Building – General Location Map



PROJECT DESCRIPTION, SCHEDULE AND FUNDING

As noted above, the City of Seat Pleasant proposes to utilize a one-story, 2,160 square foot modular building, approximately 15 feet high, for the purpose of housing the city's police force, support staff, and code enforcers. The building will not contain a prisoner holding cell, but will be used for administrative staff and officers. Five people at most will occupy the building during the first shift, three people at most will serve on the second shift, and two people will be present on the third shift. All 14 of the officers will not be in the building at the same time. Exterior improvements include resurfacing of the asphalt pavement access drive and concrete sidewalk; new accessible wood

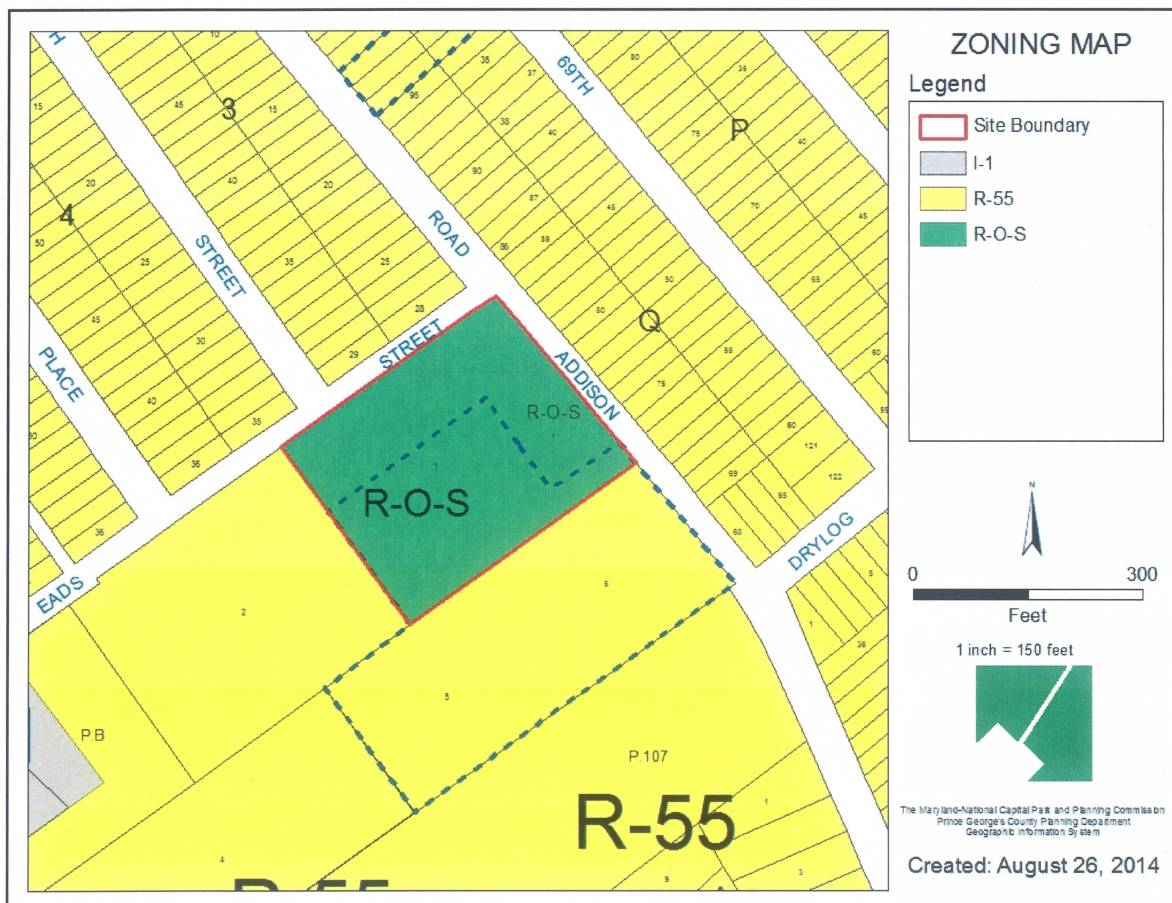
building ramp; new block retaining wall; and a new concrete pathway from the parking lot to the modular building. Vehicular site access is via Addison Road, which provides a one-way entrance/exit for the building frontage drop-off. The proposed site development requires striping for 11 parking spaces (including 2 ADA-designated parking spaces for the disabled). A public transportation bus stop is located in front of the building on Addison Road.

The proposed modular police station is expected to be open and operational by the end of 2014. The project schedule outline is as follows:

Approval of Building Permit:	October 16, 2014
Use and Occupancy Certification:	October 31, 2014
Grand Opening:	November 10, 2014

Funding is provided through the City of Seat Pleasant Budget (capital outlay and speed camera funds).

Map 3 – Proposed Modular Police Station Building – Zoning Map



**[The following pages contain
The M-NCPPC staff comments on the above project description.]**

ANALYSIS OF PROJECT IMPACT AREAS

The Maryland-National Capital Park and Planning Commission, Prince George's County Planning Department staff has reviewed the proposed modular police station building and provided the following comments:

1. ENVIRONMENTAL ASSESSMENT

A portion of this site was previously included in Special Exception, SE-110. No regulated environmental features are located on-site. The predominant soils found to occur according to the U.S. Department of Agriculture (USDA) Natural Resource Conservation Service (NRCS) Web Soil Survey (WSS) include the Christiana-Downer-Urban land complex. According to available information, Marlboro clay is not found to occur on this property; however, Christiana complexes are. According to information obtained from the Maryland Department of Natural Resources Natural Heritage Program, there are no rare, threatened, or endangered species found to occur on or in the vicinity of this property. No Forest Interior Dwelling Species (FIDS) are mapped on-site.

The site has frontage on Addison Road, which is a designated collector and not regulated for noise. The use as office space for the police force, support staff, and code enforcers is not expected to be a noise generator. The site does not front on a scenic or historic road. The site is located within the Established Communities of the Growth Policy Map and Environmental Strategy Area 1 (formerly the Developed Tier) of the Regulated Environmental Protection Areas Map as designated by the 2014 *Approved Plan Prince George's 2035 General Plan*. According to the 2005 *Approved Countywide Green Infrastructure Plan*, the site contains a small Network Gap Area within the network mapped on the southwestern corner. This area of green infrastructure is associated with a mapped off-site stream. The site is located within the Lower Beaverdam Creek watershed, within the Potomac River basin. The site's topography is generally flat, ranging from 146 feet in elevation on the southern portion of the site to 132 feet in elevation at the corner of Addison Road and Eads Street.

Environmental Review

The project qualifies for a standard exemption from the Woodland and Wildlife Habitat Conservation Ordinance because it contains less than 40,000 square feet of existing woodland. The applicant must obtain an exemption letter prior to the issuance of any local permit for the site.

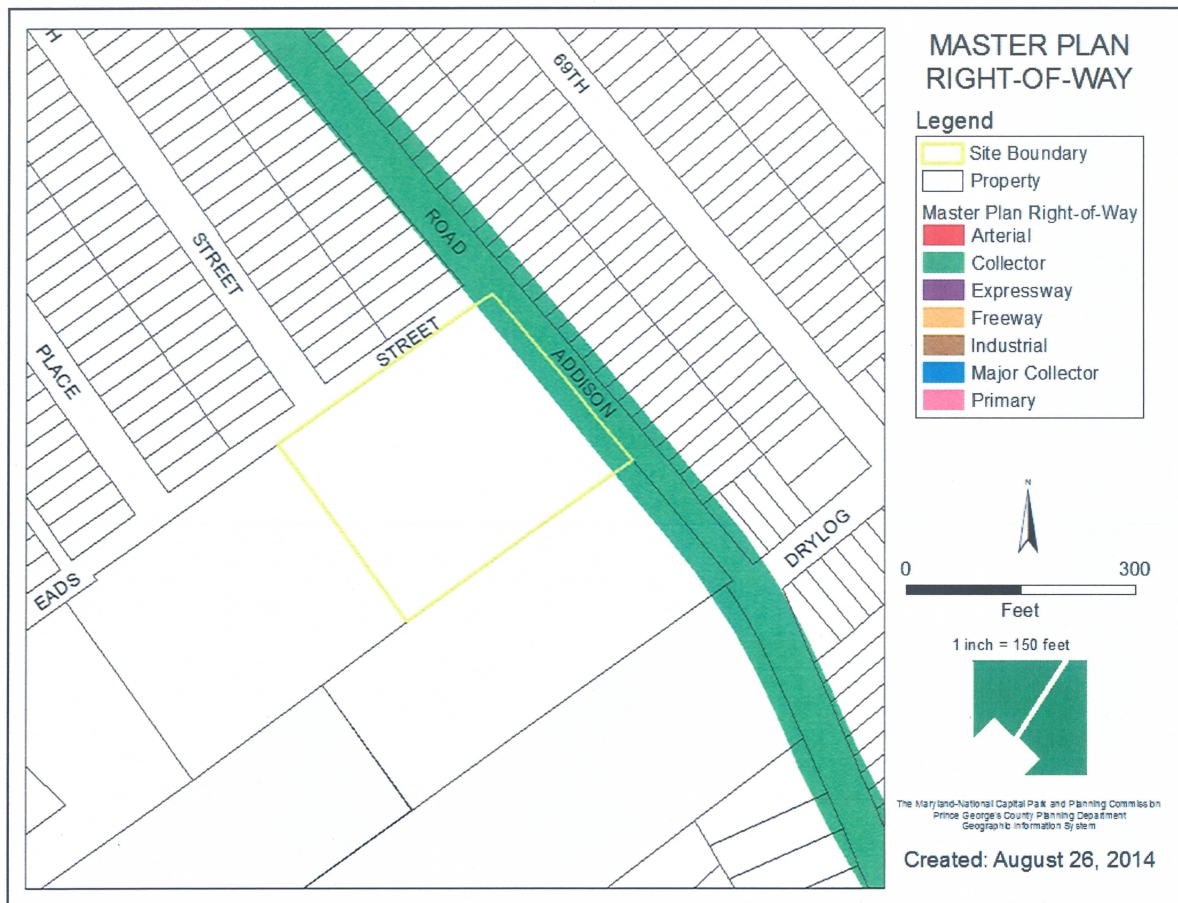
The project qualifies for a Natural Resource Inventory Equivalency Letter (NRI-EL) because the application qualifies for a standard exemption from the Prince George's County Woodland and Wildlife Habitat Conservation Ordinance and no regulated environmental features are located on-site. The applicant must obtain an equivalency letter for the County's Stormwater Management application process.

All construction is required to be conducted in accordance with erosion control and stormwater runoff laws and regulations to prevent any adverse effects on water quality. The site plan submitted with the application shows the use of on-site bioretention for stormwater management purposes. The stormwater management design is required to be reviewed and approved locally by the Department of Permitting, Inspections and Enforcement (DPIE) and a determination of the use of Environmental Site Design, to the maximum extent practicable in accordance with current state regulations, will be made with the DPIE approval. The Erosion and Sediment Control design is required to be reviewed and approved by the Prince George's County Soil Conservation District.

2. TRANSPORTATION ASSESSMENT

No subdivision application is associated with this proposal. Road frontage improvements are not proposed, and the proposal does not conflict with the 2009 *Approved Countywide Master Plan of Transportation* recommendations for Addison Road, which is a master planned collector road with a recommended right-of-way of 70-80 feet, two travel lanes, and bicycle lanes (see Map 4).

Map 4 – Master Plan Right-of-Way Map



Transportation Projects and Planned Facilities

There are no planned transportation or transit facilities adjacent to or abutting the property. It is approximately 0.68 miles from the Addison Road Metrorail Station. Sidewalk access from the subject site to the Metrorail Station is continuous, safe, and without interruption.

Traffic Impacts

No traffic impact study was provided with the application. The proposal does not include road improvements. In examining the proposal, it does not appear that the project would add additional traffic to the area in any substantial way during the peak hour traffic periods. For that reason, and because the amount of parking would not be increased, it is determined that a traffic study is not needed, and the building would not increase overall traffic.

Parking

The project includes a generous amount of on-site parking for police vehicles. The parking spaces should be striped according to County standards and specifications, and ADA accessible parking spaces should be provided at amounts that are prescribed in the zoning ordinance for similar uses.

Sidewalks

The proposal indicates that adequate sidewalks exist along Addison Road. Accessible sidewalk ramps have been constructed at the street-level sidewalk. Sidewalks that are on the site provide access to the entrances of the

building, and sidewalks will surround the building and provide access to Addison Road. The slope of the sidewalks that lead to Addison Road is rather steep.

The building is ADA accessible via wooden ramp structures (although the site was not re-graded so as to be ADA accessible from Addison Road). Because the site was not graded to provide adequate pedestrian access to the building, it is recommended that a pedestrian staircase be constructed that connects to the sidewalks on Addison Road.

Bicycle Use

Addison Road is recommended to contain bicycle lanes in the area master plan. Bicyclists share the road with vehicles at this time, and it is recommended that the property contain bicycle parking. The applicant does not propose any changes to the frontage of the property, and it was determined that bicycle lanes will not be required to be constructed at this time. There are no capital improvement projects for bicycle lanes at the location at this time.

Crosswalks

The subject site is located on Addison Road, and sidewalks are continuous and accessible on both sides of the road. A crosswalk is located nearby and approximately 250 feet south of the subject property at Drylog Street. The crosswalk is located on one leg of the intersection across Addison Road. This crosswalk does not include a pedestrian accessible signal at this time to cross the two-lane road. The proposed use is not expected to generate large volumes of pedestrian traffic.

3. HISTORIC PRESERVATION/ARCHEOLOGY

The proposed project will not impact any Prince George's County historic sites, resources, or known archeological resources. No historic properties have been identified on or adjacent to the property.

4. ECONOMIC ASSESSMENT

There are no comments on economic impacts of the proposed project in the immediate neighborhood and the County as a whole.

5. CONSISTENCY WITH DEVELOPMENT/REGULATORY STANDARDS

The property is surrounded by a passive park and single-family detached homes that are zoned R-55 (One Family Detached Residential Zone).

Location, Access, and Internal Circulation

Safe pedestrian access should be provided to the building entrance from all parking areas via sidewalks.

Landscaping

Decorative, native, landscape plantings, to include shade trees, ornamental trees and shrubs, should be placed all around the building in order to soften its appearance, integrate it into the site, and screen the mechanical equipment on the southern façade. All landscaping materials should be native species for minimal maintenance and maximum durability.

Other

The project should incorporate Leadership in Energy and Environmental Design (LEED) techniques in the design and functioning of the new building, such as encouraging recycling of office trash, usage of low-flow water fixtures, installation of a programmable thermostat, and using lights with sensor switches.

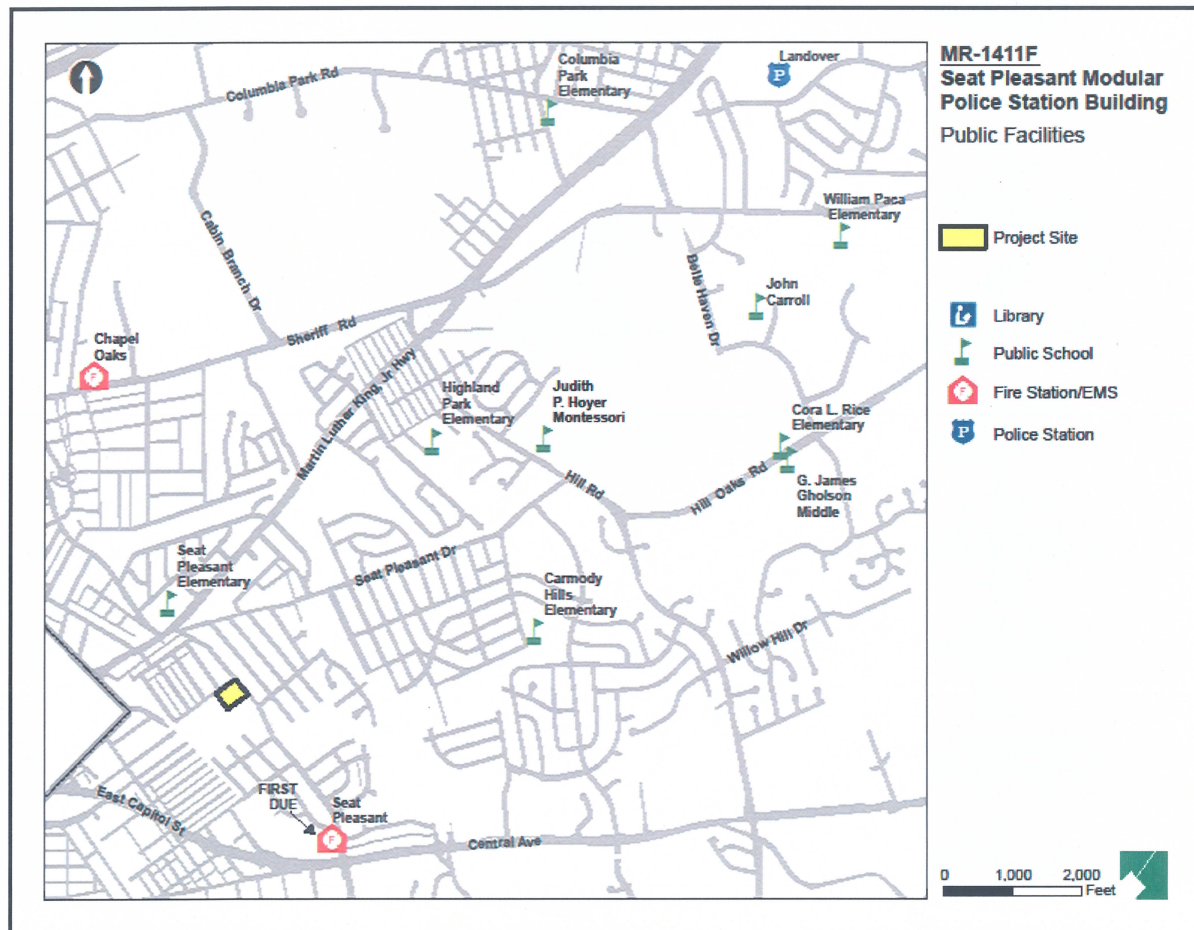
6. CONSISTENCY WITH APPROVED PLANS

The application is consistent with the 2010 *Approved Subregion 4 Master Plan*, which recommends institutional land uses for this site.

7. EXISTING PUBLIC FACILITIES

Map 5 shows the locations of existing public facilities. The next closest police station is located in Landover. The Seat Pleasant police force does not have a permanent location, which the modular building will provide.

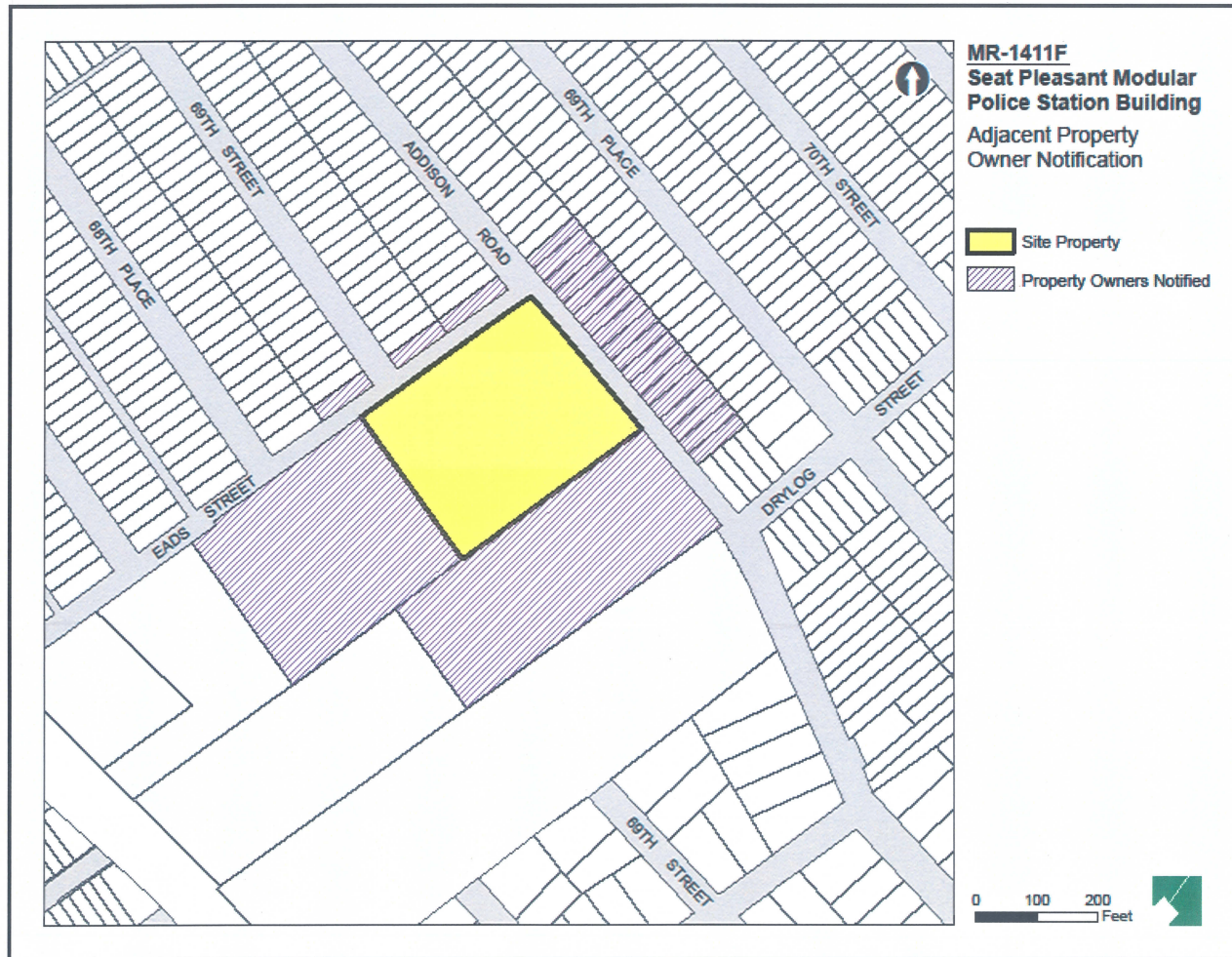
Map 5 – Public Facilities



8. COMMUNITY OUTREACH

Notification letters were mailed to adjoining property owners and civic associations informing them of the project and Planning Board hearing date (see Map 6). Staff did not receive any inquiries from any adjacent property owners inquiring about impacts on their properties.

Map 6 – Adjacent Properties



9. STAFF RECOMMENDATIONS

- The applicant should reuse existing pavement to reduce the need for further paving, remove unnecessary paving and use pervious paving for all new pavement needed.
- The applicant is encouraged to include a direct pedestrian connection from the building to the sidewalk.
- Aesthetics of the building should be enhanced through the incorporation of additional materials and colors to add visual interest, such as the addition of shutters and other architectural details to emphasize existing doors and windows; the use of enhanced cornice treatments to improve the roofline; and the addition of decorative, native, landscape plantings to provide better integration of the building into the site and screen the mechanical equipment on the southern façade. These improvements will provide a more visually and

architecturally attractive building, inspiring civic pride and commanding respect for the important policing functions that take place within its walls.

- The applicant should incorporate Leadership in Energy and Environmental Design (LEED) techniques in the design and functioning of the new building, such as encouraging recycling of office trash, usage of low-flow water fixtures, installation of a programmable thermostat, and using lights with sensor switches.
- The proposed building should be enhanced by the incorporation of site improvements such as a public plaza or gathering space, including specialty paving, seating and landscaping and an aesthetically pleasing identification sign, constructed of high quality architectural materials.
- The site should include two U-shaped bicycle parking spaces anchored into a concrete base.
- Because the site is not graded for accessible pedestrian access, it is recommended that a pedestrian staircase be constructed to provide safe access for pedestrians from Addison Road to the building. This recommendation is subject to modification and approval by the Department of Permitting, Inspections and Enforcement (DPIE).
- It is recommended that vehicle parking spaces be striped according to County standards and specifications, and that American with Disabilities Act (ADA) accessible parking spaces be provided at amounts that are prescribed in the zoning ordinance for similar uses.
- The project qualifies for a standard exemption from the Woodland and Wildlife Habitat Conservation Ordinance because the site contains less than 40,000 square feet of existing woodland. The applicant must obtain an exemption letter prior to the issuance of any local permit for the site.
- The project qualifies for a Natural Resource Inventory Equivalency Letter because the application qualifies it for a standard exemption from the Prince George's County Woodland and Wildlife Habitat Conservation Ordinance since no regulated environmental features are located on-site. The applicant must obtain an equivalency letter for the County's Stormwater Management application process.
- All construction is required to be conducted in accordance with erosion control and stormwater runoff laws and regulations to prevent any adverse effects on water quality. The site plan submitted with the application shows the use of on-site bioretention for stormwater management purposes. The stormwater management design is required to be reviewed and approved locally by the Department of Permitting, Inspections and Enforcement (DPIE) and a determination of the use of Environmental Site Design, to the maximum extent practicable in accordance with current state regulations, will be made with the DPIE approval. The Erosion and Sediment Control design is required to be reviewed and approved by the Prince George's County Soil Conservation District.

STAFF MEMOS

October 7, 2014

TO: Fatimah Hasan, Planner Coordinator, Special Projects Section

VIA: Katina Shoulars, Planning Supervisor, Environmental Planning Section

FROM: Megan Reiser, Planner Coordinator, Environmental Planning Section

SUBJECT: MR-1411F; City of Seat Pleasant Modular Police Building – 6011 Addison Road

The Environmental Planning Section has reviewed the information provided by the applicant for the City of Seat Pleasant Modular Police Building located at 6011 Addison Road. The following comments are provided for your consideration.

Site Description

The site is located at the southwest quadrant of the intersection of Addison Road and Eads Street. The property is zoned R-O-S and totals 2.16 acres. A portion of the site was previously included in Special Exception, SE-110. No regulated environmental features are located on-site. The predominant soils found to occur according to the U.S. Department of Agriculture (USDA) Natural Resource Conservation Service (NRCS) Web Soil Survey (WSS) include the Christiana-Downer-Urban land complex. According to available information, Marlboro clay is not found to occur on this property; however, Christiana complexes are. According to information obtained from the Maryland Department of Natural Resources Natural Heritage Program, there are no rare, threatened, or endangered species found to occur on or in the vicinity of this property. No Forest Interior Dwelling Species (FIDS) is mapped on-site. The site has frontage on Addison Road, which is a designated collector and not regulated for noise. The use as office space for the police force, support staff, and code enforcers is not expected to be a noise generator. The site does not front on a scenic or historic road. The site is located within the Established Communities of the Growth Policy Map and Environmental Strategy Area 1 (formerly the Developed Tier) of the Regulated Environmental Protection Areas Map as designated by 2014 *Approved Plan Prince George's 2035 Approved General Plan*. According to the approved 2005 *Approved Countywide Green Infrastructure Plan*, the site contains a small Network Gap Area within the network mapped on the southwestern corner. This area of green infrastructure is associated with a mapped off-site stream. The site is located within the Lower Beaverdam Creek watershed, within the Potomac River Basin. The site's topography is generally flat, ranging from 146 feet in elevation on the southern portion of the site to 132 feet in elevation at the corner of Addison Road and Eads Street.

Environmental Review

The proposal is for the construction of a modular police station building and associated improvements.

The project qualifies for a standard exemption from the Woodland and Wildlife Habitat Conservation Ordinance because the site contains less than 40,000 square feet of existing woodland. The applicant must obtain an exemption letter prior to the issuance of any local permit for the site.

The project qualifies for a Natural Resource Inventory Equivalency Letter (NRE-EL) because the application qualifies for a standard exemption from the Prince George's County Woodland and Wildlife Habitat Conservation Ordinance and no regulated environmental features are located on-site. The applicant must obtain an equivalency letter for the County's Stormwater Management application process.

All construction is required to be conducted in accordance with erosion control and stormwater runoff laws and regulations to prevent any adverse effects on water quality. The site plan submitted with the application shows the use of on-site bioretention for stormwater management purposes. The stormwater management design is required to be reviewed and approved locally by the Department of Permitting, Inspections and Enforcement (DPIE) and a determination of the use of Environmental Site Design to the maximum extent practicable in accordance with current state regulations will be made with the DPIE approval. The Erosion and Sediment Control design is required to be reviewed and approved by the Prince George's County Soil Conservation District.

Thank you for the opportunity to comment on the City of Seat Pleasant Modular Police Building, located at 6011 Addison. If you have questions regarding these comments, please contact the Environmental Planning Section at 301-952-3650.



THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION

Prince George's County Planning Department
Countywide Planning Division, Transportation Planning Section

(301) 952-3680
www.mncppc.org

October 8, 2014

MEMORANDUM

TO: Hasan, Fatimah, Planner Coordinator, Special Projects Section, Countywide Planning Division

FROM: Daniel Janousek, Planner Coordinator, Transportation Planning Section, Countywide Planning Division

SUBJECT: MR 1411F City of Seat Pleasant Modular Police Station Building

The Transportation Planning Section has reviewed the subject proposed. It involves the approval of a building to house the City of Seat Pleasant Modular Police Station Building.

Master Planned Road Rights-Of-Way

No subdivision application is associated with this proposal. Road frontage improvements are not proposed, and the proposal does not conflict with the 2009 *Approved Countywide Master Plan of Transportation* recommendations for Addison Road, which is a master planned collector road with a recommended right-of-way of 70-80 feet, two travel lanes, and bicycle lanes.

Transportation Projects and Planned Facilities

There are no planned transportation or transit facilities adjacent to or abutting the property. It is approximately 0.68 miles from the Addison Road Metrorail Station. Sidewalk access from the subject site to the Metrorail Station is continuous, safe, and without interruption.

Traffic Impacts

No traffic impact study was provided with the application. The proposal does not include road improvements. In examining the proposal, it does not appear that the project would add additional traffic to the area in any substantial way during the peak hour traffic periods. For that reason, and because the amount of parking would not be increased, it is determined that a traffic study is not needed, and the building would not increase overall traffic.

Parking

The project includes a generous amount of on-site parking for police vehicles. The parking spaces should be striped according to County standards and specifications, and ADA accessible parking spaces should be provided at amounts that are prescribed in the zoning ordinance for similar uses.

Sidewalks

The proposal indicates that adequate sidewalks exist along Addison Road. Accessible sidewalk ramps have been constructed at the street-level sidewalk. Sidewalks that are on the site provide access to the entrances of the building, and sidewalks will surround the building and provide access to Addison Road. The slope of the sidewalks that lead to Addison Road is rather steep.

The building is ADA accessible via wooden ramp structures (although the site was not re-graded so as to be ADA accessible from Addison Road). Because the site was not graded to provide adequate pedestrian access to the building, it is recommended that a pedestrian staircase be constructed that connects to the sidewalks on Addison Road.

Bicycle Use

Addison Road is recommended to contain bicycle lanes in the area master plan. Bicyclists share the road with vehicles at this time, and it is recommended that the property contain bicycle parking. The applicant does not propose any changes to the frontage of the property, and it was determined that bicycle lanes will not be required to be constructed at this time. There are no capital improvement projects for bicycle lanes at the location at this time.

Crosswalks

The subject site is located on Addison Road, and sidewalks are continuous and accessible on both sides of the road. A crosswalk is located nearby and approximately 250 feet south of the subject property at Drylog Street. The crosswalk is located on one leg of the intersection across Addison Road. This crosswalk does not include a pedestrian accessible signal at this time to cross the two-lane road. The proposed use is not expected to generate large volumes of pedestrian traffic.

Conclusion

The Transportation Planning Section has reviewed the referral, and determines that the proposed plan for the modular building will not increase overall traffic in the immediate area or have a negative impact on the pedestrian and bicycle environment. The proposal does not conflict with the area and functional master plans.

There are no capital improvement projects for bicycle lanes at this time for the roadway. The applicant does not propose any changes to the frontage of the property, and it has been determined that bicycle lanes cannot be constructed at this time. Bicyclists currently share the Addison Road space with motorists. It is recommended that the property contain bicycle parking spaces. The proposed building location would not interfere with any future capital improvement projects for bicycle lanes or other road improvements.

Recommendations

The proposal generally provides safe vehicle, bicycle, and pedestrian access and circulation. In order for the pedestrian access to be adequate safe and efficient, the following recommendations need to be made:

1. It is recommended that the property contain two u-shaped bicycle parking spaces anchored into a concrete base.
2. Because the site is not graded for accessible pedestrian access, it is recommended that a pedestrian staircase be constructed to provide safe access for pedestrians from Addison Road to the building. This recommendation is subject to modification and approval by Department of Permitting, Inspections and Enforcement (DPIE).
3. It is recommended that vehicle parking spaces be striped according to County standards and specifications, and that Americans with Disabilities Act (ADA) accessible parking spaces should be provided at amounts that are prescribed in the zoning ordinance for similar uses.

October 2, 2014

MEMORANDUM

TO: Fatimah Hasan, Planner Coordinator, Special Projects Section Countywide Planning Division

VIA: Scott Rowe, Planner Coordinator, Community Planning Division

FROM: John Wooden, Senior Planner, Community Planning Division

SUBJECT: **Mandatory Referral – 1411F Proposed Seat Pleasant Police Modular Building**

DETERMINATIONS

General Plan: The application is consistent with the 2014 *Plan Prince George's 2035 Approved General Plan*.

Master Plan: The application is consistent with the 2010 *Approved Subregion 4 Master Plan* which recommends institutional land uses for this site.

BACKGROUND

Location: The property is located on the southwest corner of the intersection of Eads Street and Addison Road.

Size: 2.12 acres

Existing Uses: Modular police station

Proposal: The applicant proposes to build a retaining wall and railing approximately 10-feet in height surrounding a portion of Seat Pleasant's Modular Police Station.

Master/Sector Plan: The applicant is consistent with the 2010 *Approved Subregion 4 Master Plan* which recommends institutional land uses for this site.

Plann Area/
Community: 72/Landover and vicinity

Land Use: Institutional

Environmental:	Please see the Environmental Section's referral for details regarding relevant environmental recommendations. The property does not include any regulated areas, evaluation areas, or network gaps identified by the Green Infrastructure Plan.
Historic Resources:	No historic properties have been identified on or adjacent to the property.
Transportation:	Access is provided to Addison Road, a collector roadway recommended for two lands with a 70 to 80-foot right-of-way.
Public Facilities:	The subject property houses a modular police station.
Parks and Trails:	There are no parks on or adjacent to the subject property. However, a bike lane is proposed along Addison Road from the D.C. Line to Walker Mill Road abutting the subject property.
Aviation/ILUC:	The property is included within the Joint Base Andrews Imaginary Runway Surface Height Zone F. In this zone, the maximum height requirement is five hundred feet.
SMA/Zoning:	The 2010 <i>Approved Subregion 4 Master Plan and Sectional Map Amendment</i> retained the subject property in the R-O-S Zone.

PLANNING ISSUES

There are no planning issues.

c: Ivy A. Lewis, Chief, Community Planning Division



THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION

14741 Governor Oden Bowie Drive
Upper Marlboro, Maryland 20772
TTY: (301) 952-4366
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September 29, 2014

MEMORANDUM

TO: Fatimah Hasan, Planner Coordinator
Special Projects Section, Countywide Planning Division

VIA: Ruth Grover, Planner Coordinator, Urban Design Section, Development Review Division

FROM: Jill Kosack, Senior Planner, Urban Design Section, Development Review Division

SUBJECT: Mandatory Referral MR-1411F
Proposed Modular Police Station Building in Seat Pleasant

The Urban Design Section has reviewed materials provided in support of MR-1411F, Proposed Modular Police Station Building in Seat Pleasant, Maryland. The application proposes the construction of an approximately 15-foot high, 2,160 square foot building to house the City of Seat Pleasant's police department on approximately 2.33 acres within the R-O-S (Reserved Open Space) Zone. The city-owned site is located in the southwest quadrant of the intersection of Addison Road and Eads Street. Directly west and south of the subject site are more city-owned properties, which are vacant and developed as a passive park, respectively. To the east and north, beyond the public rights-of-way are single-family detached homes in the R-55 (One-Family Detached Residential Zone). The subject project is being reviewed as part of the Mandatory Referral review process pursuant to Maryland Annotated Code, Land Use Article, Sections 20-301 through 305 and Section 27-294 of the Prince George's County Zoning Ordinance.

The Urban Design Section offers the following recommendations for the proposed project:

1. The existing vacant site, which was previously developed, is largely paved. The Urban Design Section suggests the applicant be strongly encouraged to reuse existing pavement, reduce the need for further paving, remove unnecessary paving, and use pervious paving for all new paving on-site.
2. The applicant should be encouraged to revise the plans to include a direct pedestrian connection from the building to the sidewalk within the public right-of-way.
3. The submitted materials indicate that the proposed building has already been placed on-site. If this is the case, the Urban Design Section would suggest the following improvements to enhance the aesthetics of the building:
 - a. Visual interest should be added to the northern, western and southern facades through the incorporation of additional materials and colors.
 - b. The existing doors and windows on all facades should be emphasized through the use of architectural details, such as shutters, enhanced trim, awnings, etc.

- c. The roofline should be improved through the use of enhanced cornice treatments.
- d. Decorative, native, landscape plantings, to include shade trees, ornamental trees and shrubs, should be placed all around the building in order to soften its appearance, integrate it into the site, and screen the mechanical equipment on the southern facade.

If the fact that the building has already been placed on the site is not a limitation, the Urban Design Section suggests the building be redesigned to be more visually and architecturally attractive, inspiring civic pride and commanding respect for the important policing functions that take place within its walls.

- 4. While the building is not proposed to hold any jail facilities, it is possible the police use may create noise and light nuisances for the nearby single-family homes. The Urban Design Section suggests the police department consider the implementation of rules regarding use of flashing lights and sirens in the near vicinity of the property, so as to lessen potential negative impacts of the project on the surrounding residential areas.
- 5. Incorporate Leadership in Energy and Environmental Design (LEED) techniques in the design and functioning of the new building, such as encouraging recycling of office trash, usage of low-flow water fixtures, installation of a programmable thermostat, and using lights with sensor switches.
- 6. The proposed building should be enhanced by the incorporation of site improvements such as a public plaza or gathering space, including specialty paving, seating and landscaping and an aesthetically pleasing identification sign, constructed of high quality architectural materials.